

PROJECT NO.	SHEET

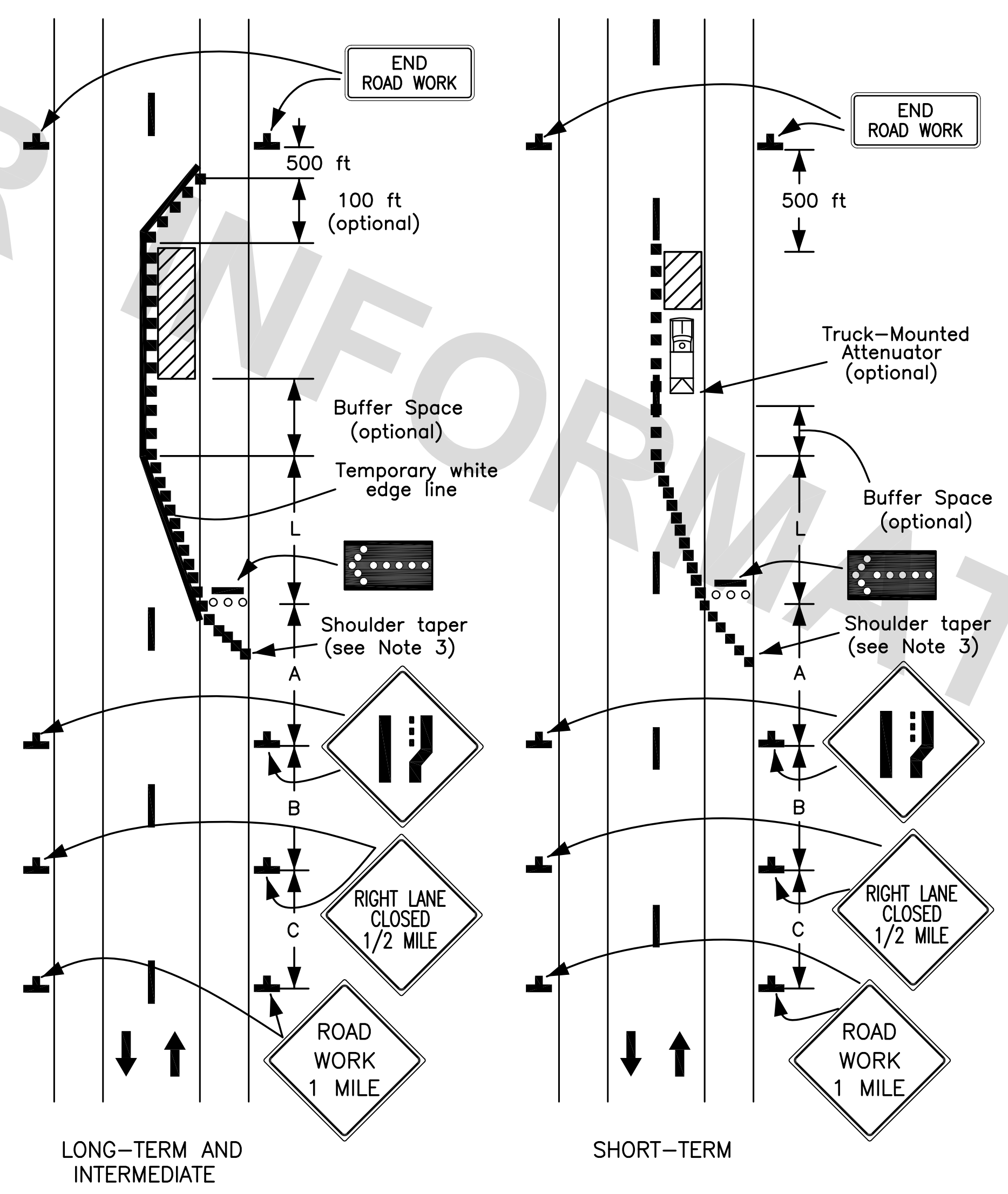


Figure TA-33
Lane Closure on Divided Highway

- NOTES:
1. This information also shall be used when work is being performed in the lane adjacent to the median on a divided highway. In this case, the LEFT LANE CLOSED signs and the corresponding Lane Ends signs shall be substituted.
 2. When a side road intersects the highway within the TTC zone, additional TTC devices shall be placed as needed.
 3. When paved shoulders having a width of Eight (8) ft or more are closed, channelizing devices should be used to close the shoulder in advance of the merging taper to direct vehicular traffic to remain within the traveled way.
 4. A truck-mounted attenuator may be used on the work vehicle and/or shadow vehicle.
 5. Where conditions permit, restricting all vehicles, equipment, workers, and their activities to one side of the roadway might be advantageous.

This sheet shall be used with Standard Plan No. 905-01 and 905-02.

NOTES:

1. When highway-rail grade crossings exit either within or in the vicinity of roadway work activities, extra care should be taken to minimize the probability of conditions being created, either by lane restrictions, flagging or other operations, where vehicles might be stopped within the highway-rail grade crossing, considered as being Fifteen (15) ft on either side of the closest and farthest rail.
2. If the queuing of vehicles across active rail tracks cannot be avoided, a uniformed law enforcement officer or flagger shall be provided at the highway-rail grade crossing to prevent vehicles from stopping within the highway-rail grade crossing (as described in Note 1), even if automatic warning devices are in place.
3. Early coordination with the railroad company should occur before work starts.
4. In the example depicted, the buffer space of the activity area should be extended up stream of the highway-rail grade crossing (as shown) so that a queue created by the flagging operation will not extend across the highway-rail grade crossing.
5. The DO NOT STOP ON TRACKS sign should be used on all approaches to a highway-rail grade crossing within the limits of a TTC zone.
6. Flashing warning lights and/or flags may be used to call attention to the advance warning signs.
7. A BE PREPARED TO STOP sign may be added to the sign series.
8. When used, the BE PREPARED TO STOP sign should be located before the Flagger symbol sign.
9. At night, flagger stations shall be illuminated, except emergencies.

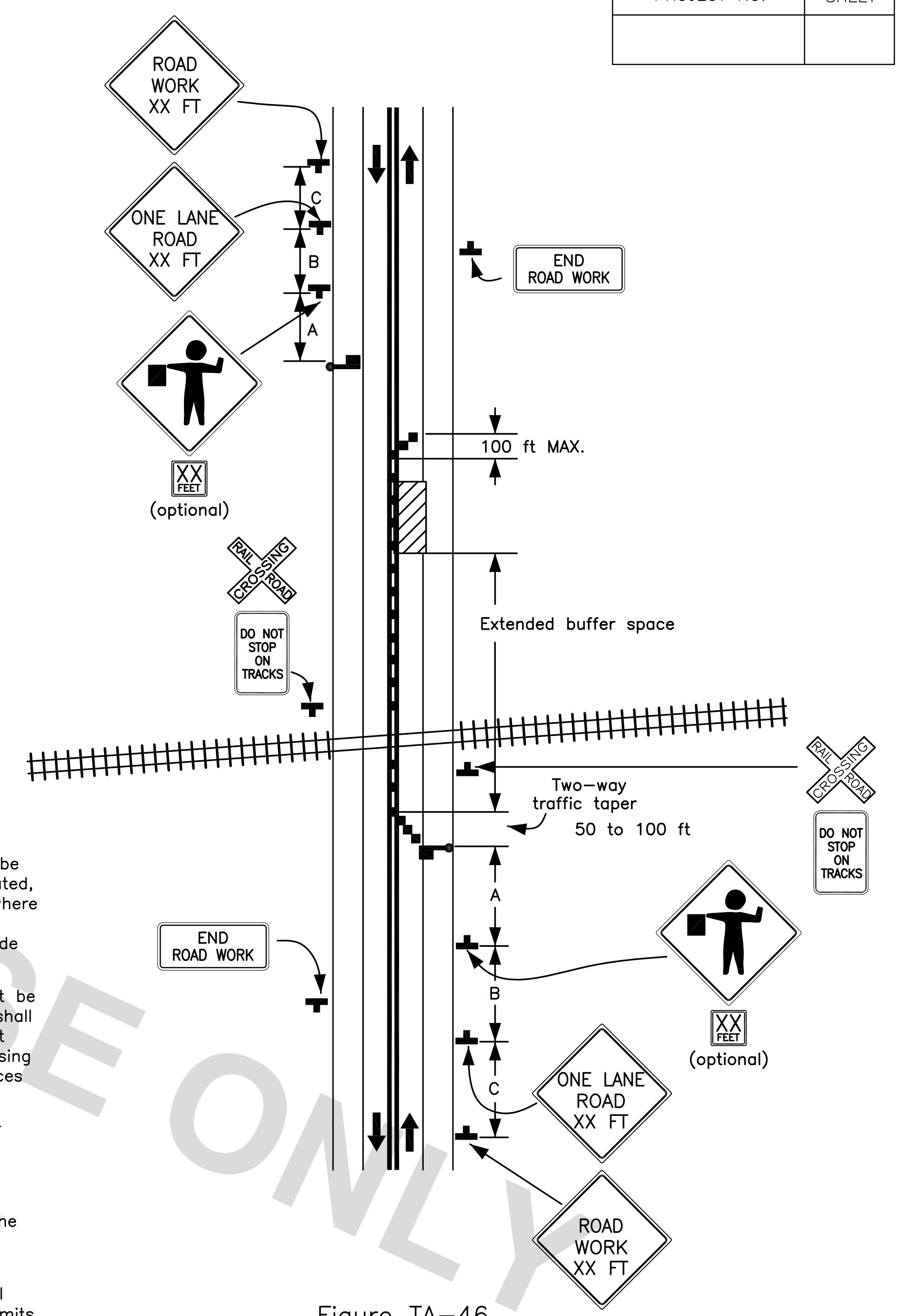
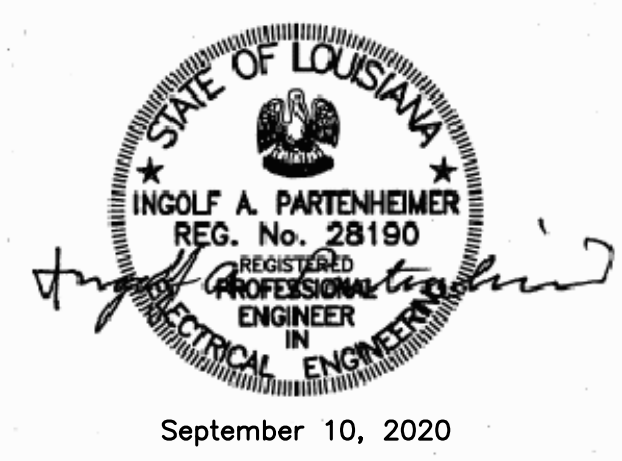


Figure TA-46
Work in Vicinity of Highway-Rail Grade Crossing



STANDARD PLAN NO. 905-11	DATED JULY 3, 2019	SHEET NO. 1 OF 1
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TEMPORARY TRAFFIC CONTROL
TYPICAL APPLICATIONS

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