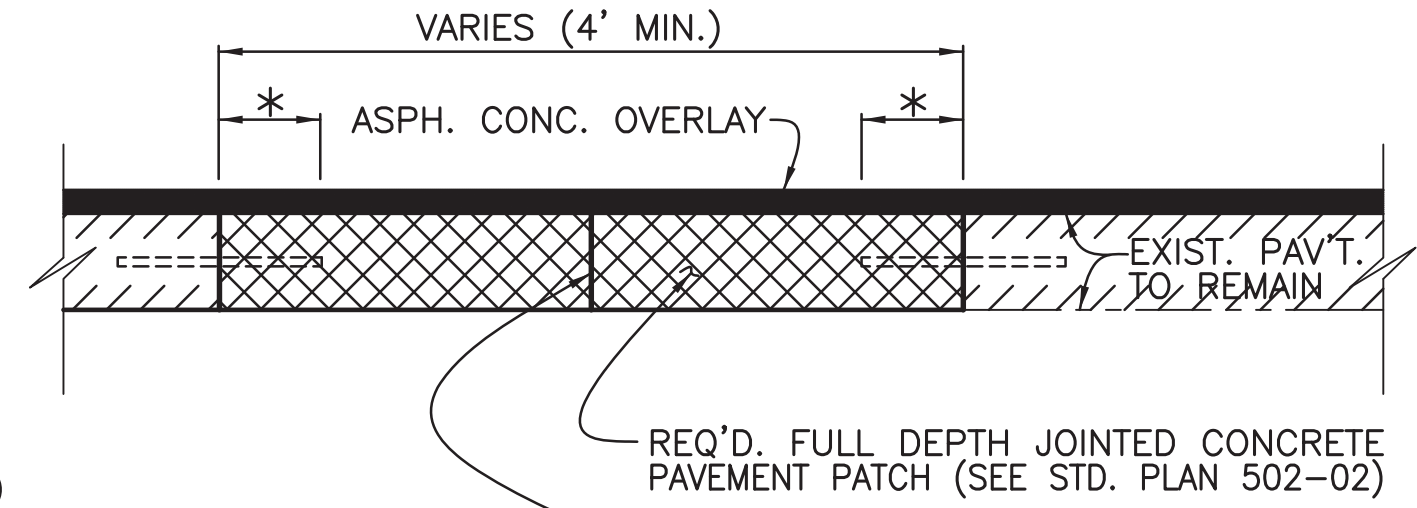
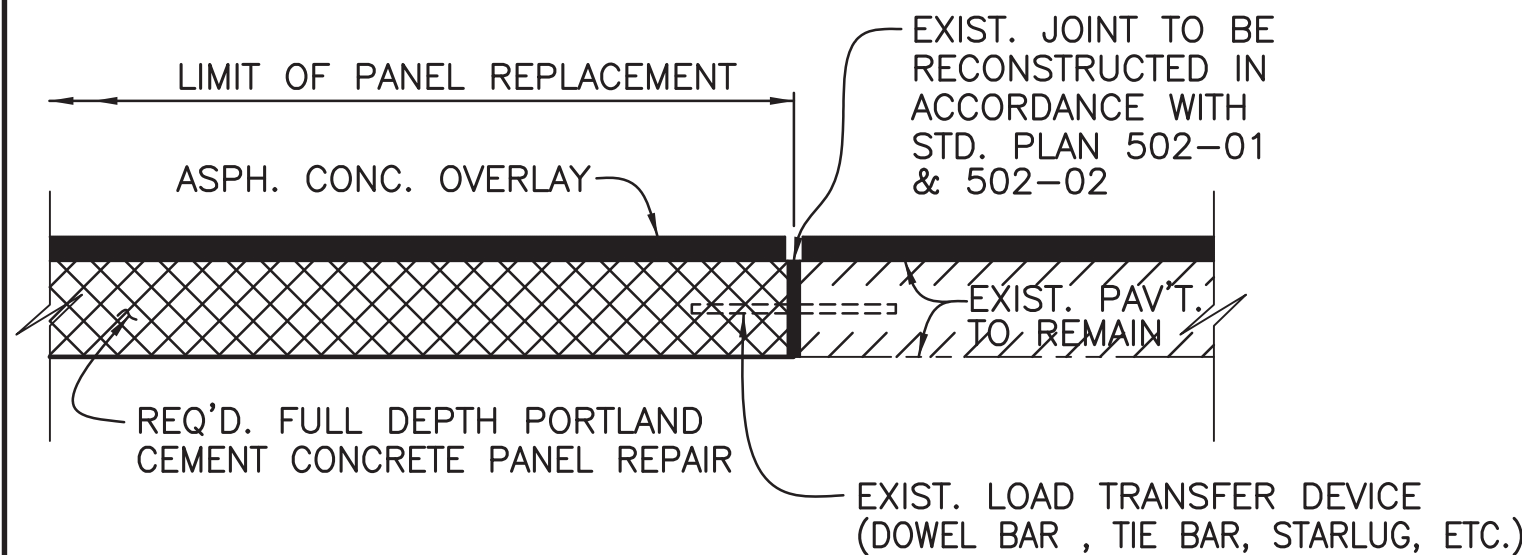


PROJECT NO.	SHEET

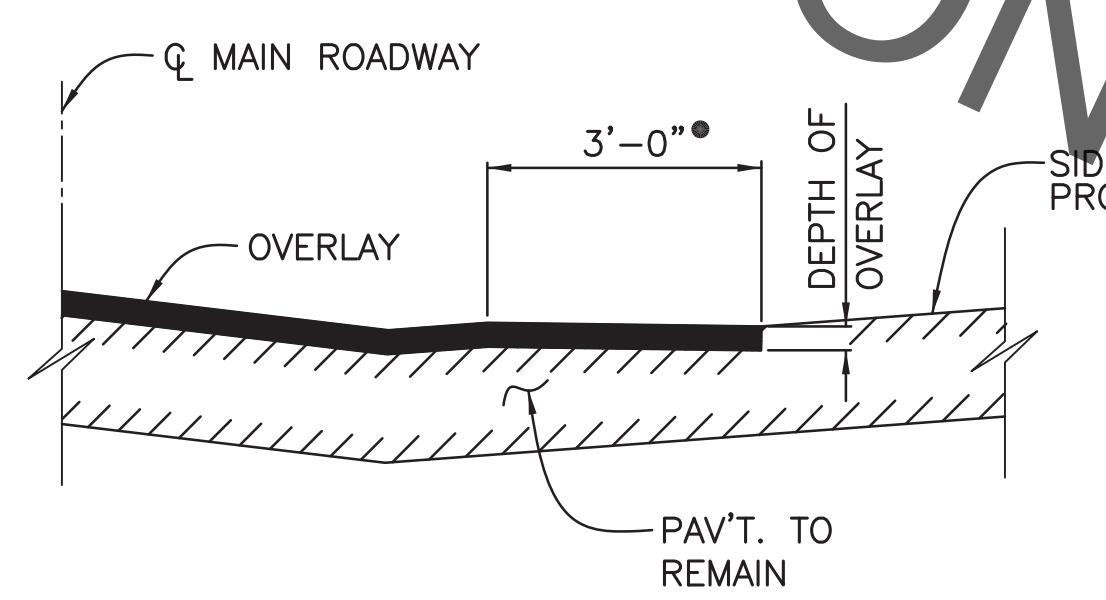
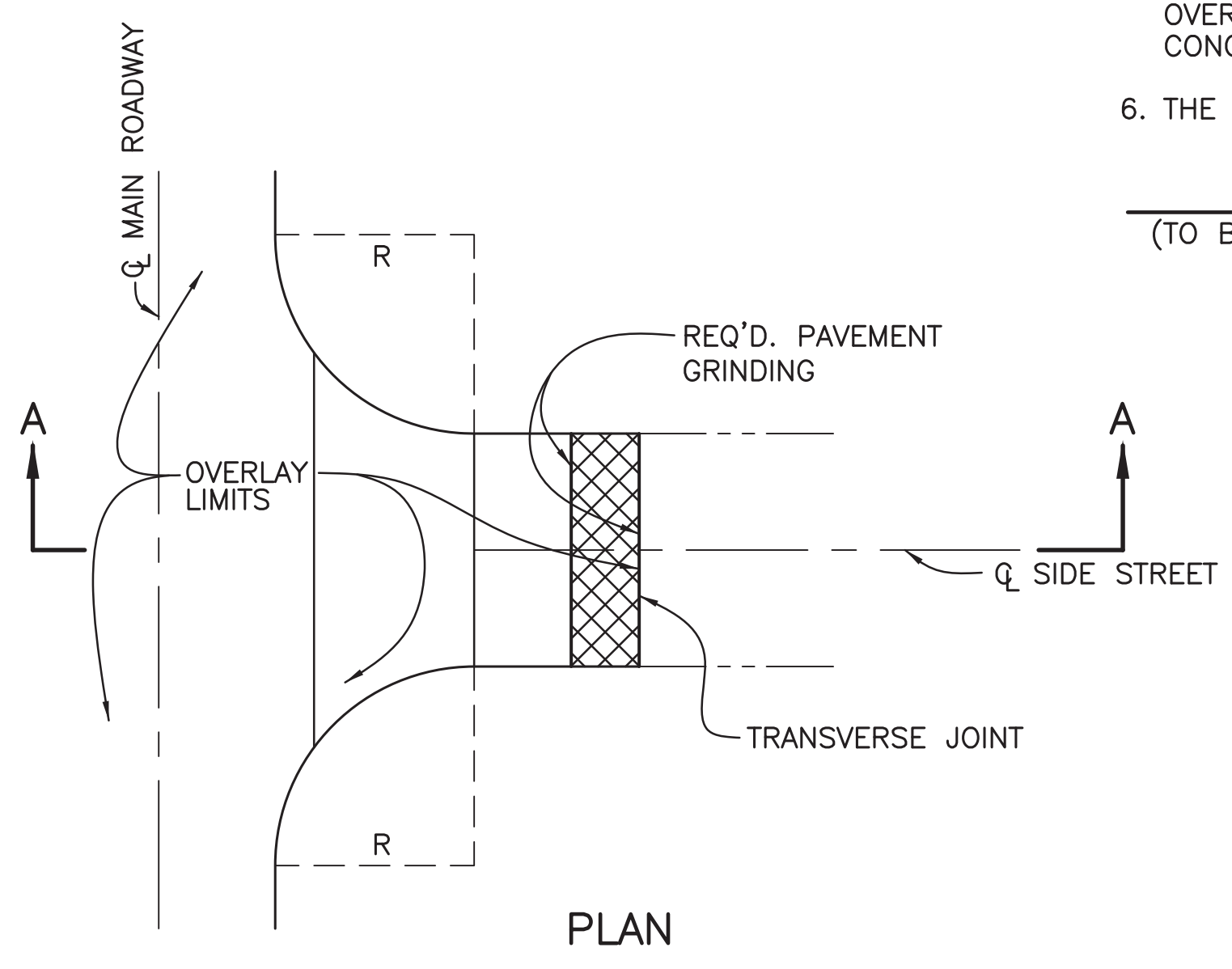


1. DURING CONCRETE REMOVAL, THE CONTRACTOR SHALL TAKE PRECAUTION TO PRESERVE THE INTEGRITY OF EXISTING LOAD TRANSFER DEVICES. (SEE LOAD TRANSFER DEVICE NOTES BELOW)
2. DETERIORATED BASE COURSE SHALL BE REMOVED AND REPLACED WITH CONCRETE. AN APPROVED BOND BREAKER SHALL BE PLACED BETWEEN THE REPAIRED BASE COURSE AND PORTLAND CEMENT CONCRETE PATCH.
3. JOINTS SHALL BE REESTABLISHED IN THE PORTLAND CEMENT CONCRETE PANEL REPLACEMENT AT THE ORIGINAL JOINT LOCATIONS. JOINTS SHALL BE SEALED. JOINTS SHALL BE SAWED AND SEALED IN THE ASPHALT CONCRETE OVERLAY. SEE DETAILS OF SAWING AND SEALING JOINTS IN ASPHALT CONCRETE OVERLAY.
4. THE PANEL REPAIR SURFACE SHALL BE A DRAG FINISH.

- JOINT TO BE RECONSTRUCTED IN ORIGINAL LOCATION IN ACCORDANCE WITH STD. PLAN 502-01
- \*1. AN 18" CLEARANCE BETWEEN THE SAWCUT AND THE NEAREST TRANSVERSE CRACK IS DESIRED. MINIMUM CLEARANCE SHALL BE 6".
  2. DURING CONCRETE REMOVAL, THE CONTRACTOR SHALL TAKE PRECAUTION TO PRESERVE THE INTEGRITY OF EXISTING LOAD TRANSFER DEVICES. (SEE LOAD TRANSFER DEVICE NOTES BELOW).
  3. DETERIORATED BASE COURSE SHALL BE REMOVED AND REPLACED WITH CONCRETE. AN APPROVED BOND BREAKER SHALL BE PLACED BETWEEN THE REPAIRED BASE COURSE AND THE PORTLAND CEMENT CONCRETE PATCH.
  4. FACE OF EXISTING CONCRETE SHALL BE COATED WITH AN APPROVED EPOXY ADHESIVE (DOD OPL 32) BEFORE PLACING CONCRETE
  5. JOINTS SHALL BE REESTABLISHED IN THE PORTLAND CEMENT CONCRETE PATCH AT THE ORIGINAL JOINT LOCATION. JOINTS SHALL BE SEALED. JOINTS SHALL BE SAWED AND SEALED IN THE ASPHALT CONCRETE OVERLAY. SEE DETAILS OF SAWING AND SEALING JOINTS IN ASPHALT CONCRETE OVERLAY BELOW.
  6. THE PANEL REPAIR SURFACE SHALL BE A DRAG FINISH.

**CONCRETE PAVEMENT PATCHING**  
(TO BE USED @ WHOLE PANEL REPLACEMENT LOCATIONS)

**CONCRETE PAVEMENT PATCHING**  
(TO BE USED @ JOINT REPAIR AND PARTIAL PANEL REPLACEMENT LOCATIONS)

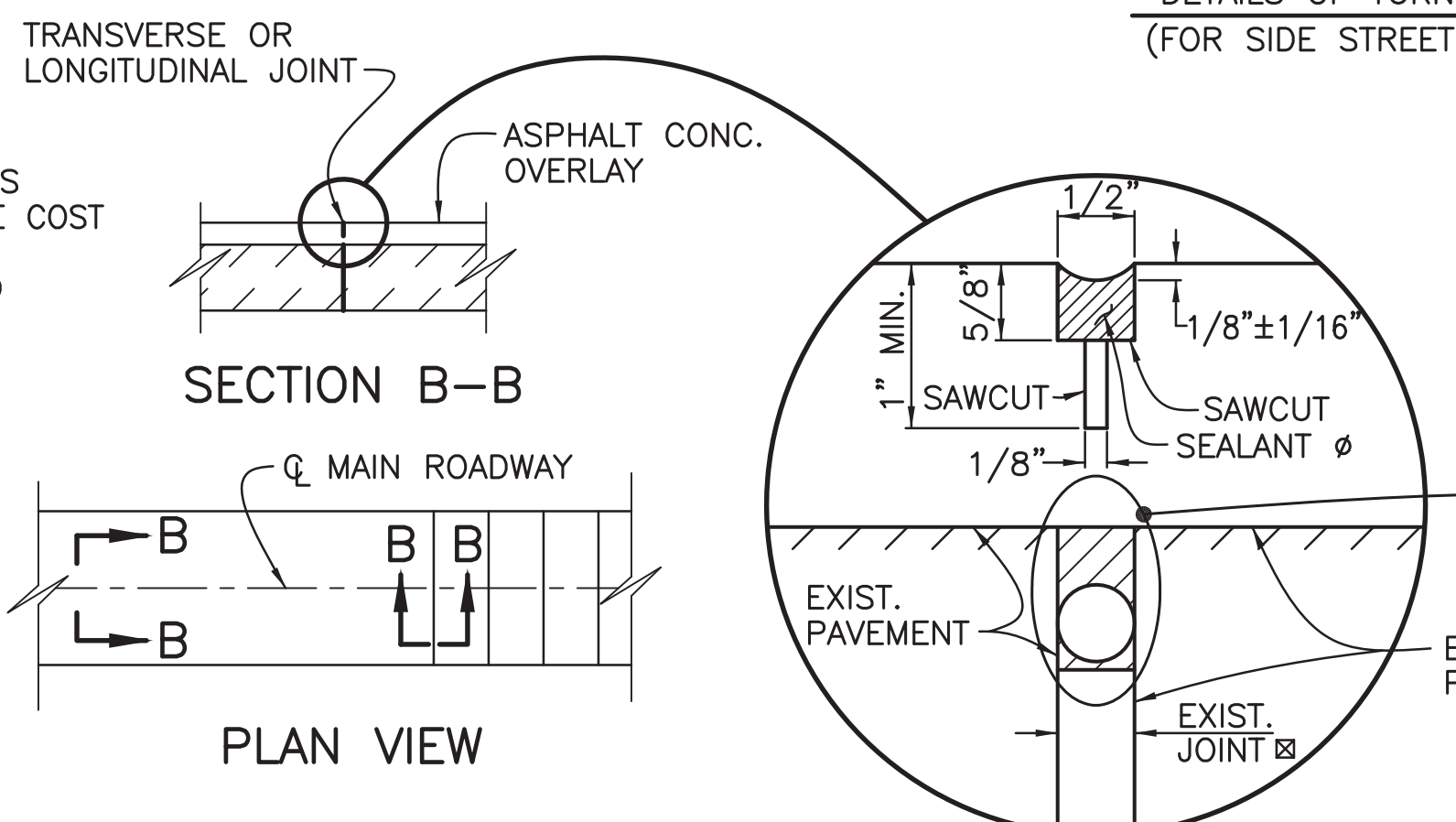


- LOCATION OF PAVEMENT GRINDING TO BE AT CURB RETURN OR NEXT TRANSVERSE JOINT AS DETERMINED BY THE ENGINEER.

**SECTION A-A**  
DETAILS OF TURNOUTS AND INTERSECTIONS  
(FOR SIDE STREETS WITHOUT OPEN DITCHES)

**LOAD TRANSFER DEVICE NOTES**

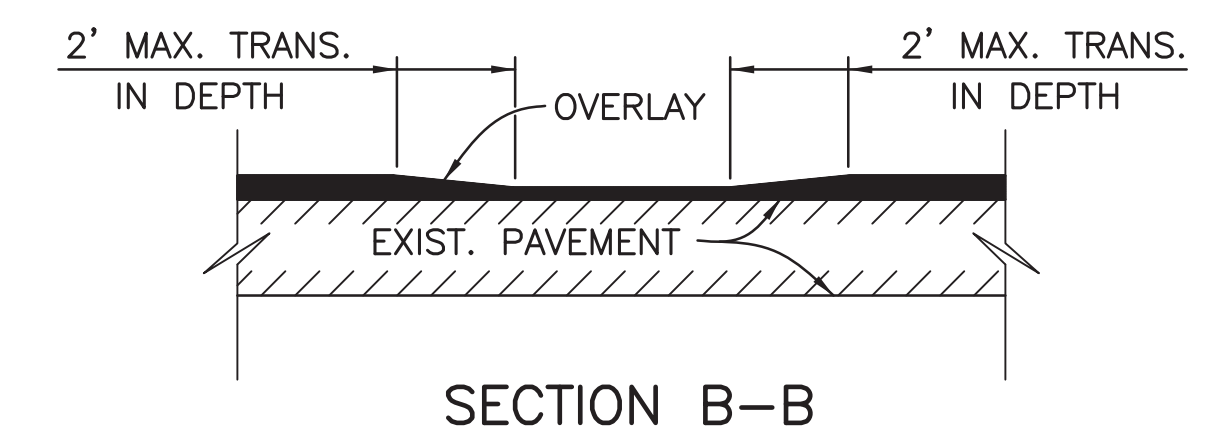
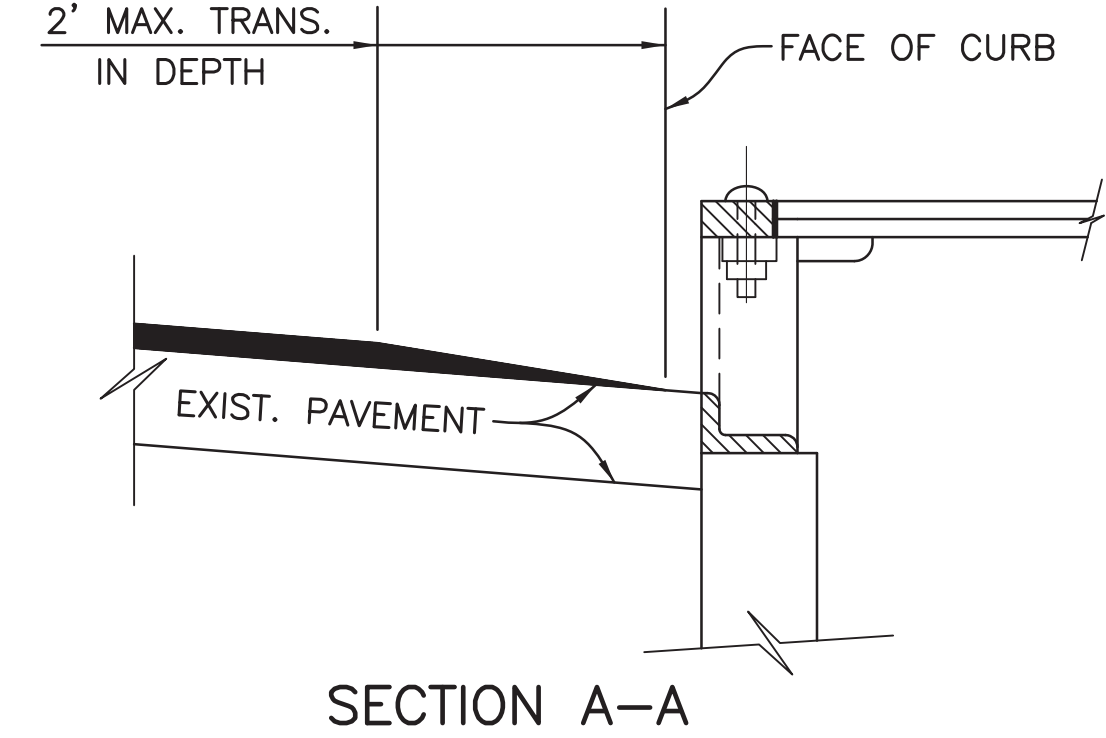
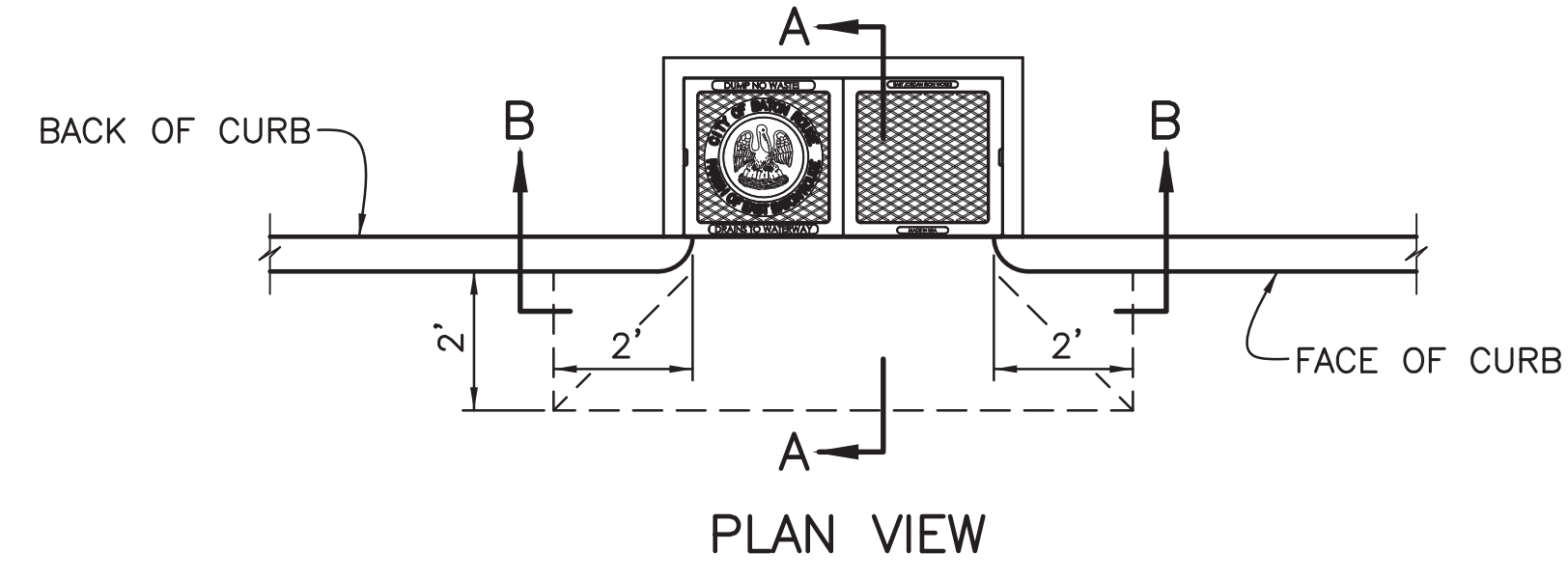
1. DOWEL BARS SHALL BE INSTALLED AT JOINT LOCATIONS WHERE EXIST. LOAD TRANSFER DEVICES HAVE FAILED. THE COST OF INSTALLATION, LABOR AND MATERIALS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE PORTLAND CEMENT CONCRETE PATCH ITEM.
2. 1 1/4"x18" PLASTIC COATED DOWEL BARS SHALL BE INSTALLED ON 12" CENTERS. THE BARS SHALL BE PLACED IN EXISTING PAVEMENT BY DRILLING 1 3/8" HOLES TO A DEPTH OF 9" AND FILLING WITH AN APPROVED EPOXY GROUT BEFORE INSERTION OF DOWEL BARS.
3. TIE BARS SHALL BE INSTALLED IN ACCORDANCE WITH STANDARD PLAN 502-01. LABOR AND MATERIALS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE PORTLAND CEMENT CONCRETE PATCH ITEM.
4. DOWEL BARS AND TIE BARS SHALL BE INSTALLED AS OUTLINED ABOVE AT THE CONTRACTOR'S EXPENSE AT LOCATIONS WHERE THE CONTRACTOR DAMAGES EXIST. LOAD TRANSFER DEVICES OR TIE BARS.



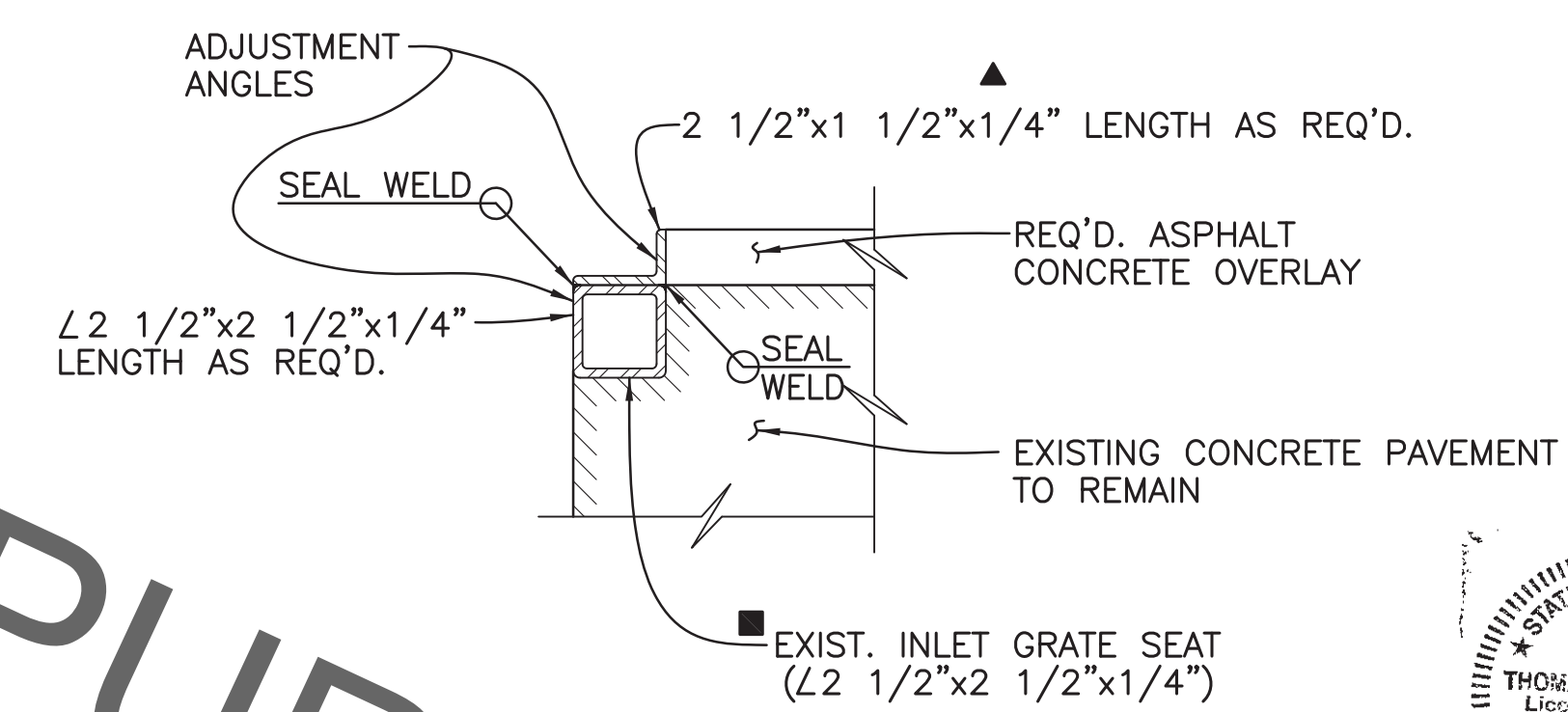
1. INCOMPRESSIBLES SHALL BE REMOVED TO A MINIMUM DEPTH OF HALF THE PAVEMENT THICKNESS FOR JOINTS WIDTHS OF 1" AND LESS AND TO FULL PAVEMENT DEPTH WHERE JOINT WIDTHS ARE OVER 1".

**DETAILS OF SAWING AND SEALING JOINTS IN ASPHALT CONCRETE OVERLAY**

- EXISTING JOINT TO BE CLEANED AND SEALED.
- SEE SPECIAL PROVISIONS FOR ALLOWED TYPE OF SEALANT.
- FOR DETAIL OF CLEANING AND RESEALING EXISTING JOINTS, SEE STANDARD PLAN 502-02.

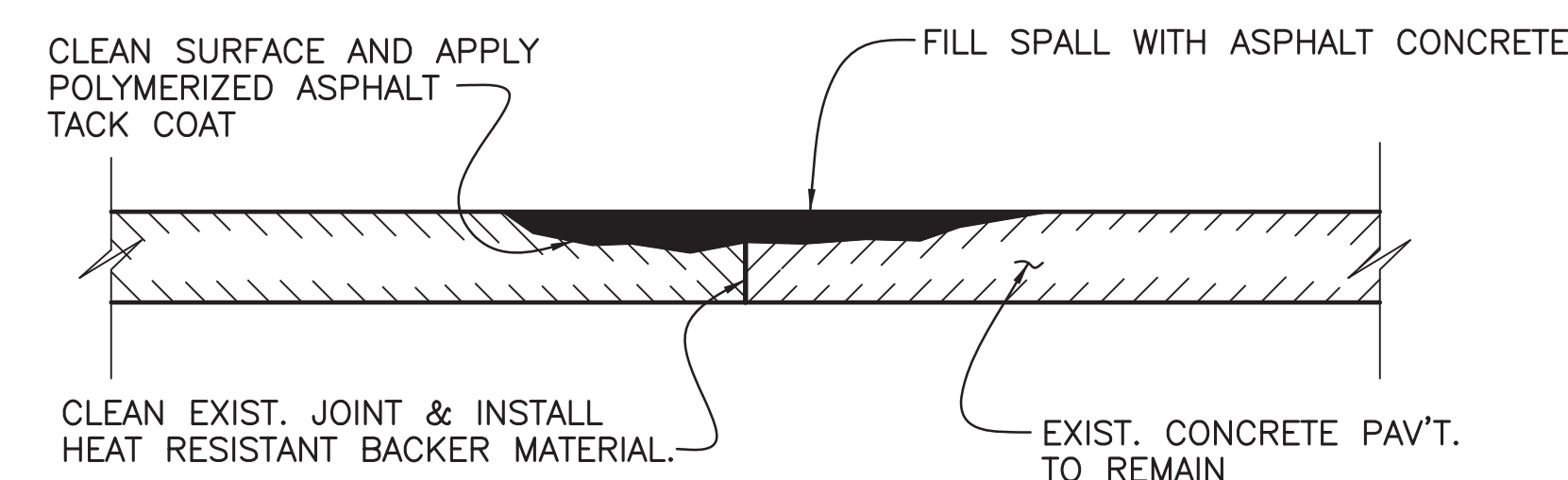


**SECTION A-A**  
**DETAIL OF SUMP IN OVERLAY AT SIDE INLET**



- NOTES:
1. STRUCTURAL STEEL TO BE A-36 GALVANIZED.
  2. ALL Ls TO BE CUT TO FIT AND WELDED AT CORNERS.
  3. ADJUSTMENT ANGLE DIMENSIONS BASED ON 2" ASPHALT CONCRETE OVERLAY THICKNESS.
  - 4. AT GRATE INLET LOCATIONS WHERE PANEL REPLACEMENT IS REQUIRED THE EXISTING INLET GRATE SEAT SHALL BE RESET TO ITS ORIGINAL ELEVATION AND ADJUSTED AS SHOWN FOR THE REQUIRED OVERLAY.
  - ▲ 5. DIMENSION SHALL BE 1/2" LESS THAN ASPHALT OVERLAY THICKNESS.

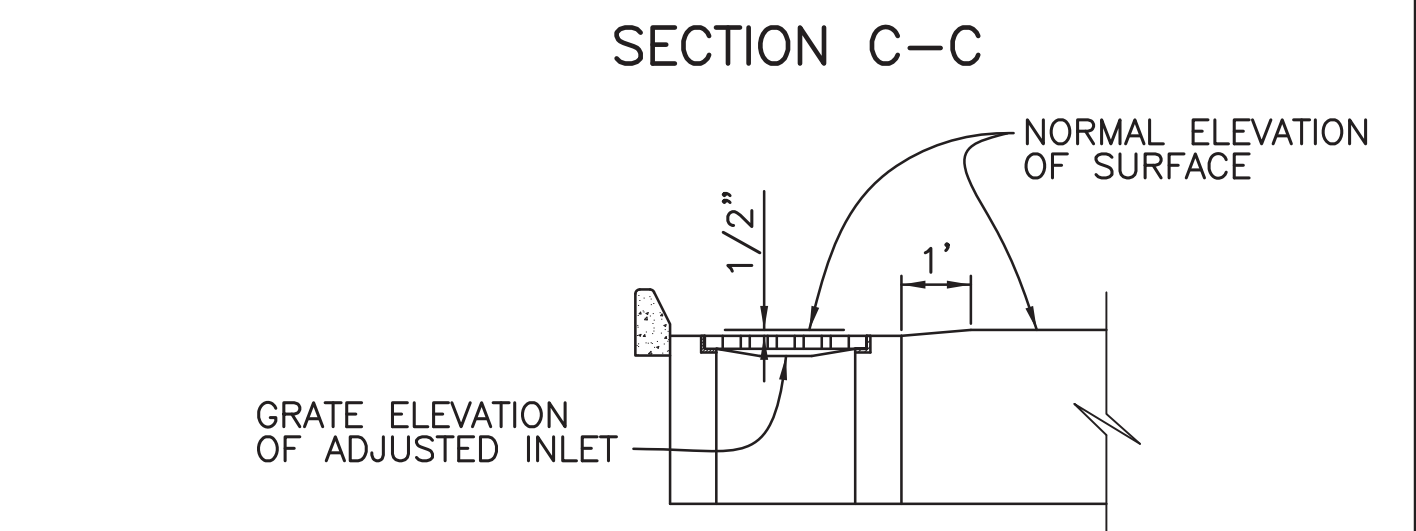
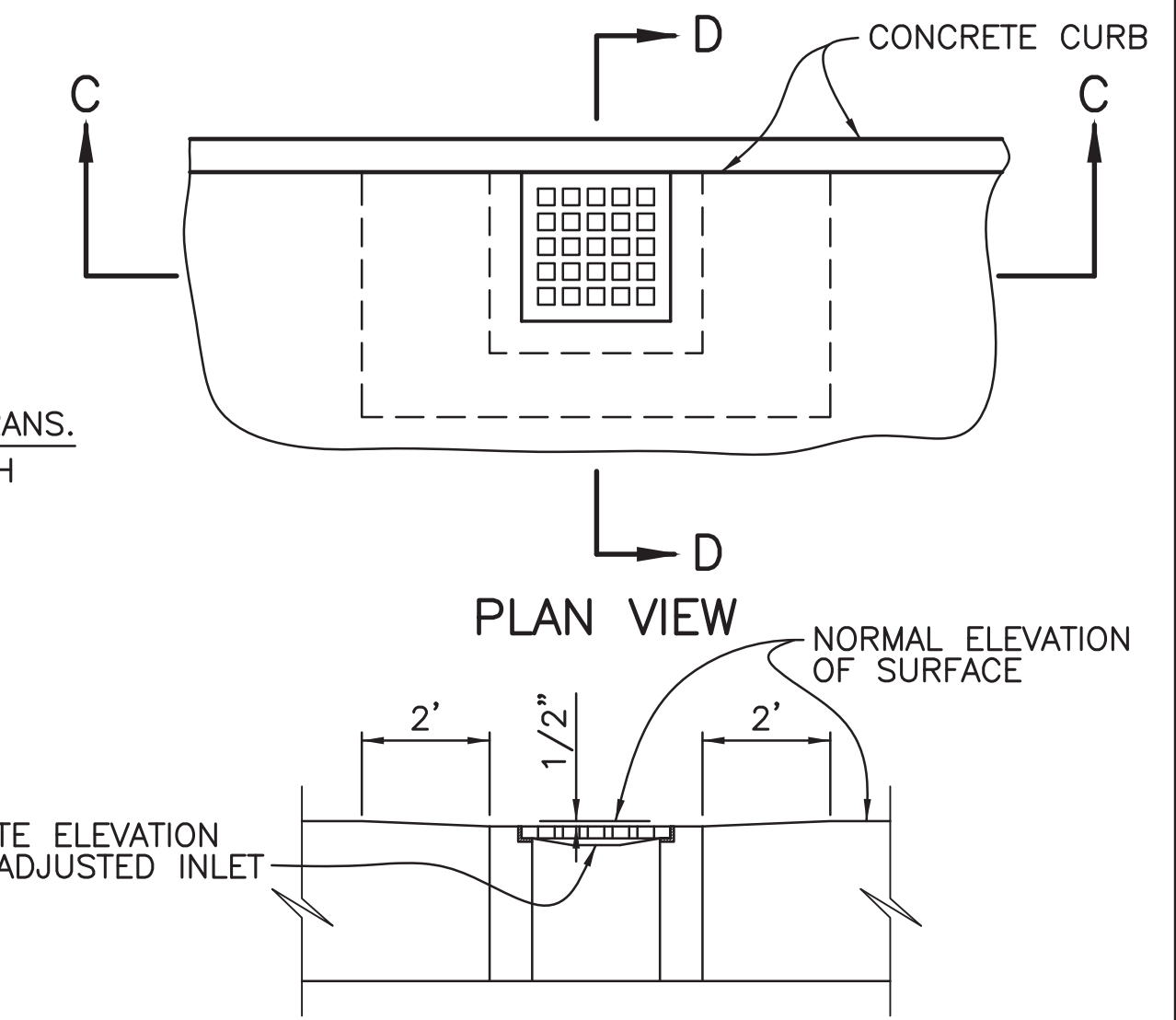
**GRATE INLET ADJUSTMENT DETAIL**



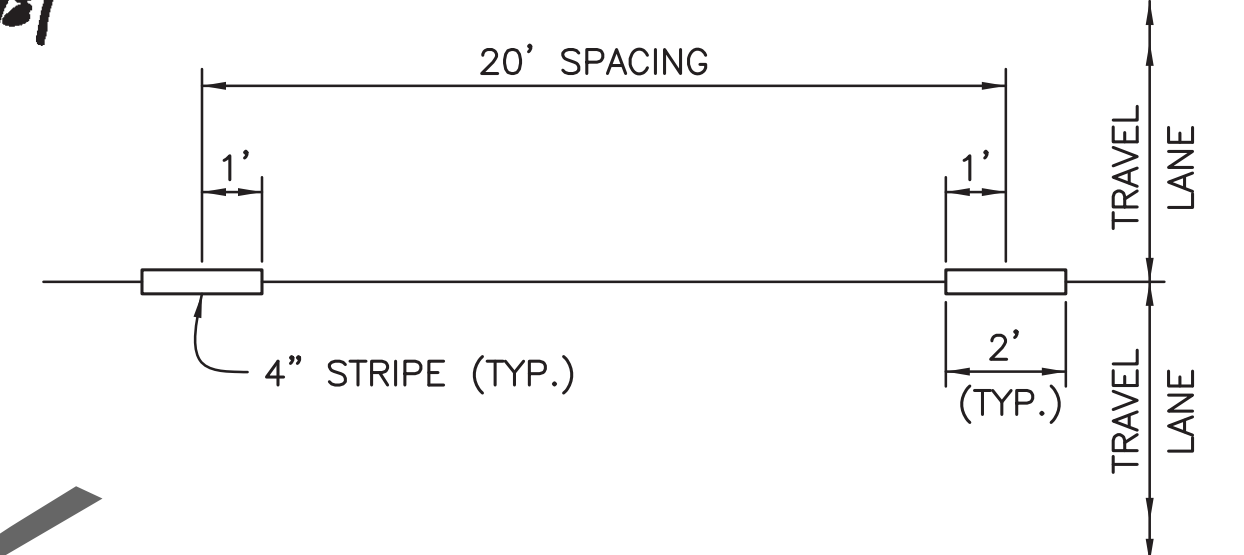
1. SURFACE PREPARATION, APPLICATION OF TACK COAT AND ASPHALT CONCRETE REQUIRED TO FILL SPALLED AREA SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE ASPHALT CONCRETE PAVEMENT ITEM.

**SPALL REPAIR DETAIL**

DATE	DESCRIPTION	BY

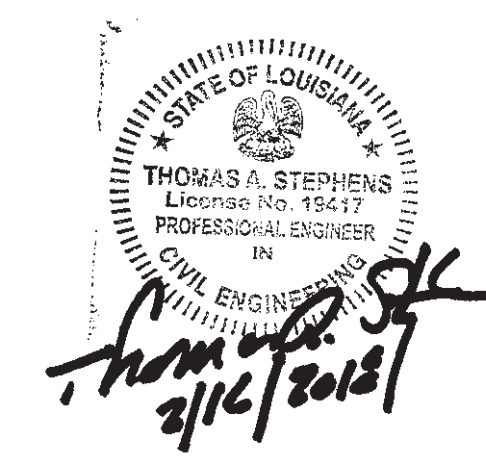


**GRATE INLET SUMP DETAIL**  
(SHOWING TRANSITION IN SURFACE ELEVATION)



- NOTE:
1. TEMPORARY PAVEMENT MARKINGS REQUIRED IN ALL OVERLAY AREAS.

**TEMPORARY PAVEMENT MARKING DETAIL**



STANDARD PLAN NO. 501-01	DATED AUGUST 6, 2008	SHEET NO. 1 OF 1
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**ASPHALT CONCRETE OVERLAY OF P.C. CONCRETE PAVEMENT**

ENGINEERING DIVISION DEPARTMENT OF PUBLIC WORKS CITY OF BATON ROUGE & PARISH OF EAST BATON ROUGE			
DESIGNED R.K. BANKS	DRAWN G. VANNICE	CHECKED R.E. ELLIS	APPROVED T. STEPHENS